

Reference: 22.030r01v04

23 June 2023

Stanton Dahl Architects
18-20 Oxford Street
EPPING NSW 2121

Attention: Jason O'Keefe

**Re: 62-66 Pank Parade, Blacktown – Proposed LAHC Seniors Living Development
Traffic Impact Statement**

Dear Jason,

TRAFFIX has been commissioned to assess the traffic impacts in support of a Development Application (DA) relating to a proposed Land and Housing Corporation (LAHC) seniors living development at 62-66 Pank Parade, Blacktown. The subject site is located within the Blacktown City Council local government area and has been assessed under that Council's controls, as well as the State Environmental Planning Policy (Housing) 2021 (SEPP).

This statement documents the findings of our investigations and should be read in the context of the Statement of Environmental Effects (SEE), prepared separately. The proposed development is considered minor and as such, will not require referral to Transport for NSW (TfNSW – formally RMS) under the provisions of State Environmental Planning Policy (Infrastructure) 2007.

➤ Site and Location

The subject site at 62-66 Pank Parade, Blacktown is located approximately 2.0 kilometres northwest of Blacktown Railway Station and is legally identified as Lot 46, 47 and 48 in DP32163. More specifically, it is situated on the southern side of the Pank Parade and Robyn Street intersection, approximately 75 metres east of McClean Street.

The site is rectangular in configuration and has a total site area of 1,670.2m². It has a northern frontage of 45.7 metres to Pank Parade, while the remaining southern boundary of 45.7 metres and eastern/western boundaries of 36.5 metres are shared with neighbouring residential properties.

The site currently accommodates a three (3) residential dwellings that each provide vehicular access onto Pank Parade along the northern frontage of the site.

A Location Plan is presented in **Figure 1**, with a Site Plan presented in **Figure 2**. Reference should be made to the Photographic Record presented in **Attachment 1**, which provides an appreciation of the general character of roads and other key attributes within proximity of the site.

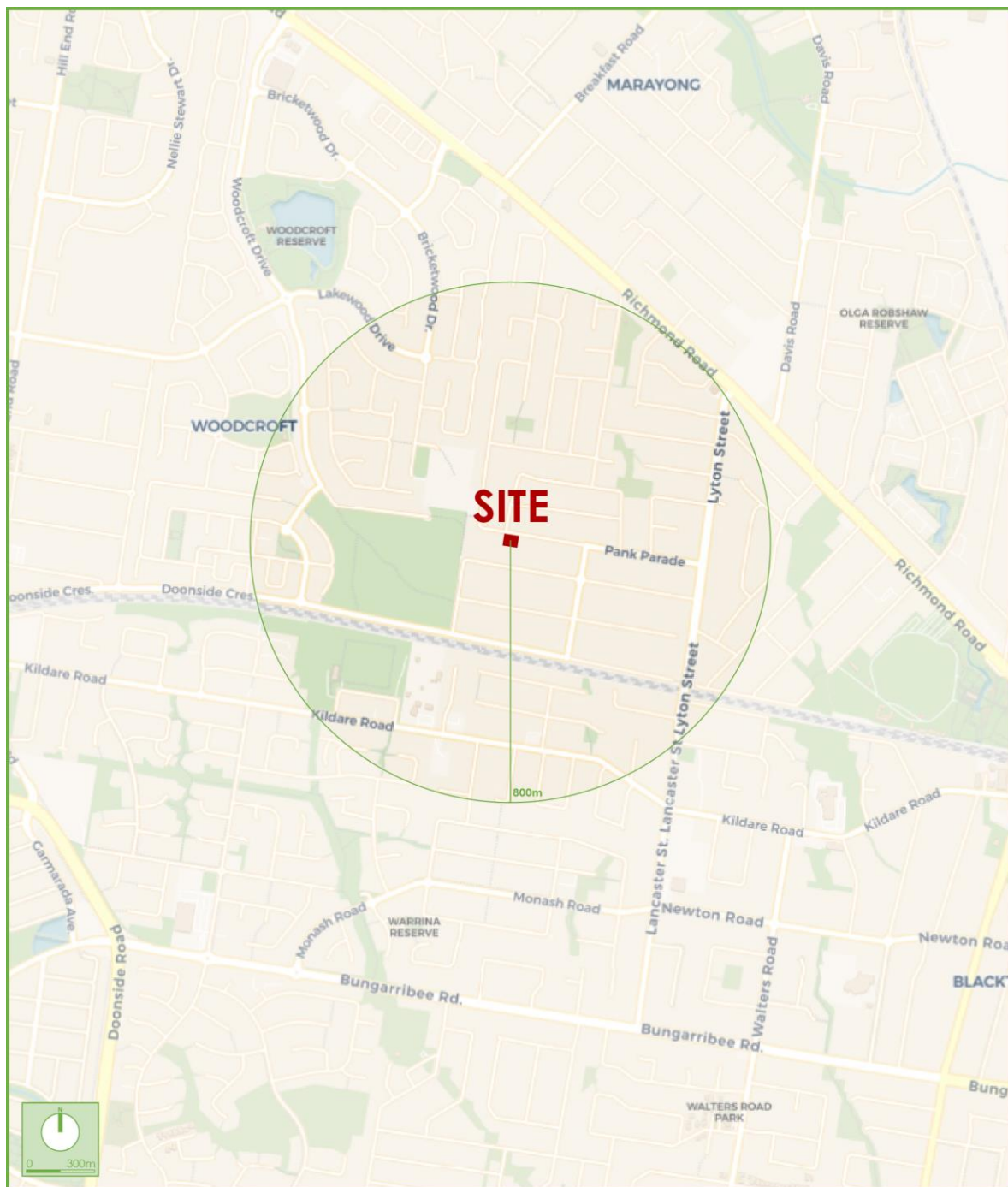


Figure 1: Location Plan

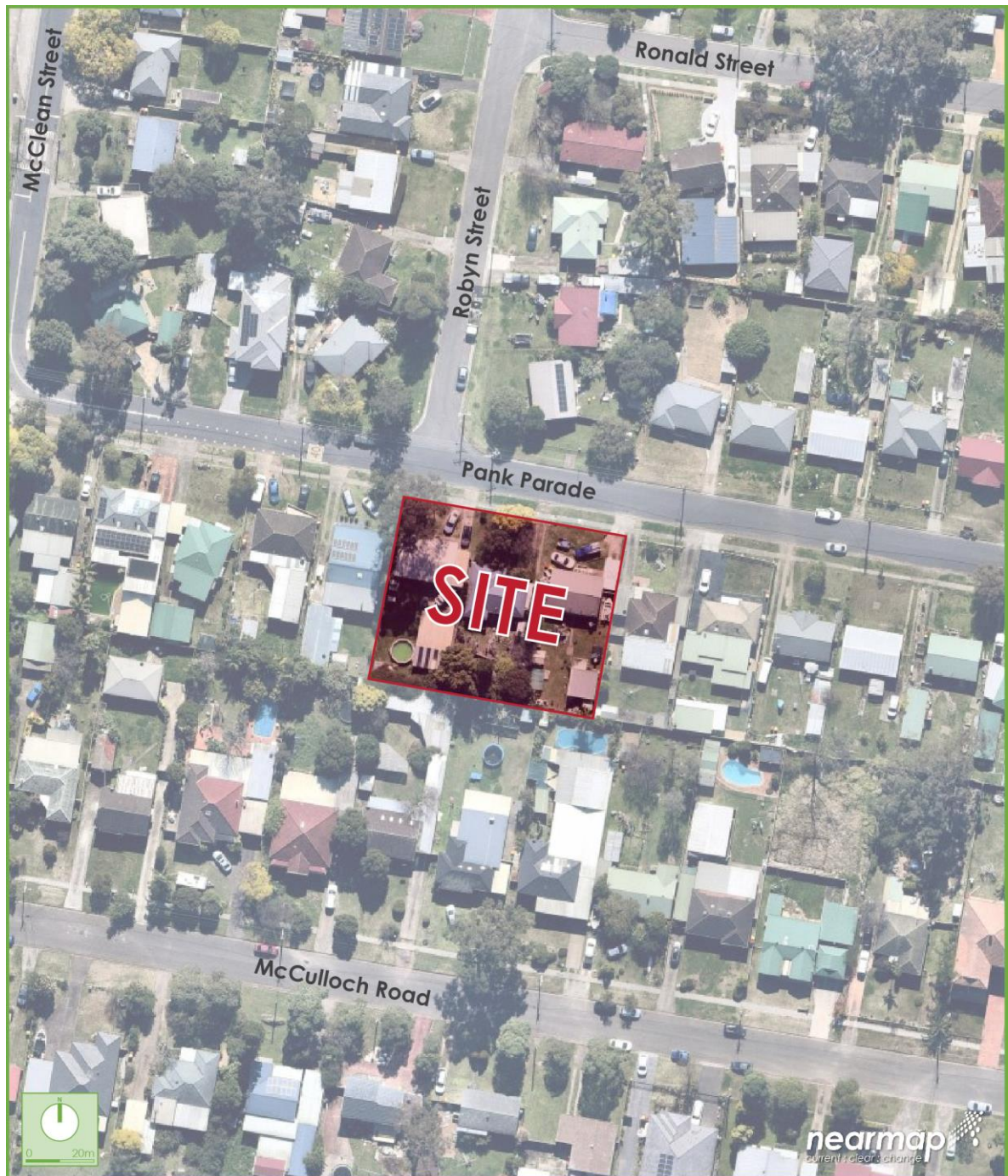


Figure 2: Site Plan

➤ Road Hierarchy

The road hierarchy in the vicinity of the site is shown in **Figure 3** with the following roads of particular interest:

- **Richmond Road:** forms part of a TfNSW Main Road (MR 643) that traverses north-south between Blacktown Road in the north and Main Street in the south. Within the vicinity of the site, it is subject to 60km/h speed zoning and accommodates two (2) lanes of traffic in each direction. Richmond Road does not permit on-street parking on either side of the road.
- **Lyton Street:** an unclassified regional road (RR 7465) that traverses north-south between Richmond Road in the north and Lancaster Street in the south. It is subject to 60km/h speed zoning and generally accommodates two (2) lanes of traffic in each direction. Lyton Street permits sections of unrestricted on-street parking along both sides of the road.
- **Pank Parade:** a local road that traverses east-west between Lyton Street in the east and McClean Street in the west. It is subject to 50km/h speed zoning and accommodates a single lane of traffic in each direction. Pank Parade permits unrestricted on-street parking along both sides of the road.
- **Robyn Street:** a local road that traverses east-west between Pank Parade near Leonard Street in the east and Pank Parade near McClean Street in the west. It is subject to 50km/h speed zoning and accommodates a single lane of traffic in each direction. Robyn Street permits unrestricted on-street parking along both sides of the road.

It can be seen from **Figure 3** that the site is conveniently located with respect to the main arterial roads serving the region, being Lyton Street and Richmond Road. As such, traffic can effectively be distributed onto the wider road network, minimising traffic impacts.

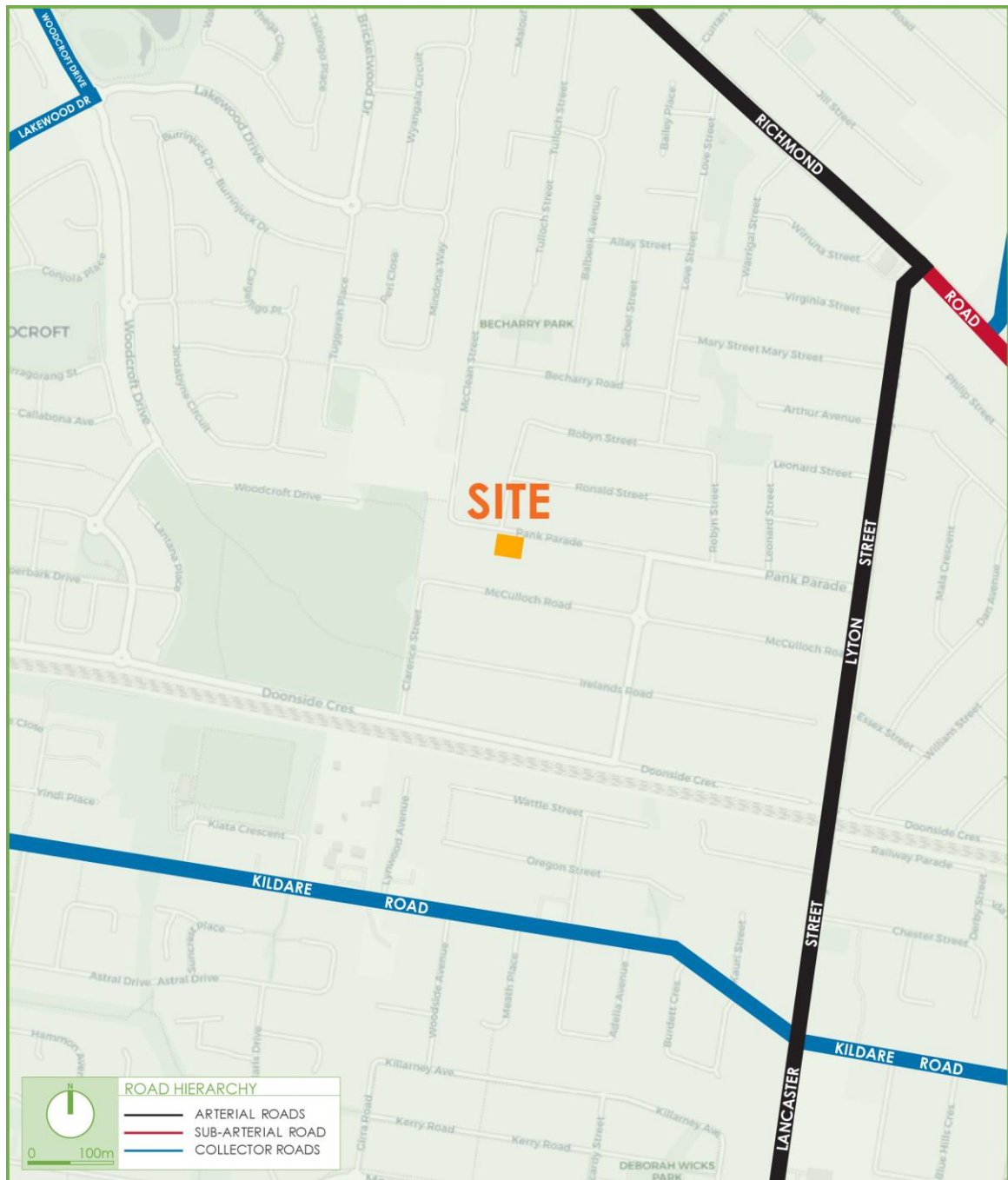


Figure 3: Road Hierarchy

Public Transport

The subject site is within optimal walking distance (400 metres) of several bus stops operating within the locality, with the closest situated on Pank Parade, along the northern frontage of the site. These bus stops are presented in Figure 4, with the routes and approximate frequencies outlined in Table 1 below.

Table 1: Bus Routes and Service Frequency

Bus No	Route	Service Frequency		
		Weekdays	Saturdays	Sundays
753	Blacktown to Doonside	Every 60-minutes (9:00am-6:00pm)	Every 60-minutes (8:30am-4:30pm)	Every 120-minutes (10:00am-4:00pm)
756	Mount Druitt to Blacktown via Plumpton and Woodcroft	Every 30-minutes (5:30am-Midnight)	Every 30-minutes (6:00am-Midnight)	Every 60-minutes (8:00am-11:00pm)

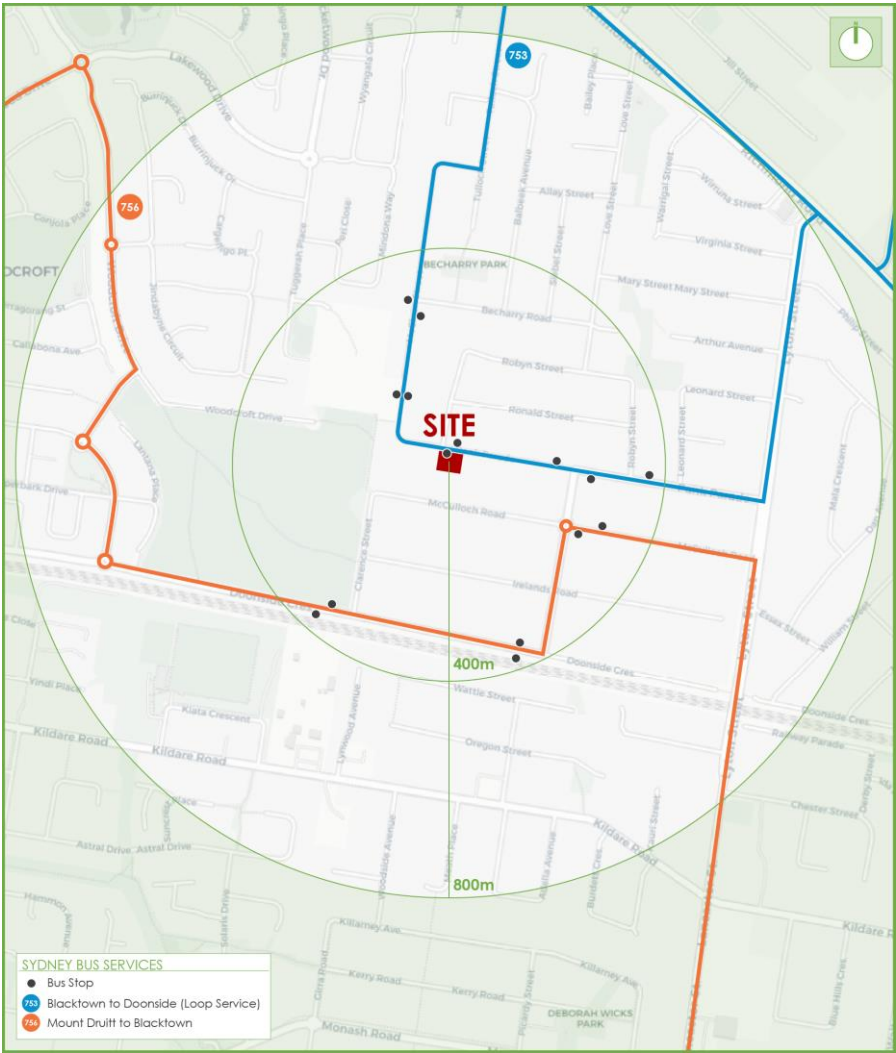


Figure 4: Public Transport

Description of Proposed Development

A full description of the proposed development can be found in the SEE, prepared separately. In summary, the development for which approval is now sought comprises the following components:

- Demolition of the existing residential dwellings;
- Construction of a LAHC seniors living development with a total of 12 independent living units (ILUs), including:
 - 6 x one-bedroom dwellings; and
 - 6 x two-bedroom dwellings.
- Provision for a total of six (6) car parking spaces, including:
 - 2 x accessible parking spaces within the at-grade carpark;
 - 3 x standard parking spaces within the at-grade carpark; and
 - 1 x standard parking space in the form of a carport.
- Provision of two (2) vehicular accesses onto Pank Parade.

Reference should be made to the plans submitted separately to Council that are presented at a reduced scale in **Attachment 2**.

Parking Requirements

Car Parking

The SEPP (Housing) 2021 Part 5, Division 7, Clause 108(j) provides the minimum car parking rates for ILU developments made by a social housing provider, as outlined in **Table 2** below.

Table 2: SEPP Car Parking Rates and Provisions

Type	No. of Dwellings	SEPP Car Parking Rates	Parking Required ^[1]	Parking Provided
Independent Living Units – Social Housing Provider				
One-bedroom	6	1 space per 5 dwellings	1.2	6
Two-bedroom	6		1.2	
TOTAL			2.4 (2)	6

[1] – Rounded to the nearest whole number.

It can be seen from **Table 2** that the proposed development is nominally required to provide a minimum of two (2) car parking spaces, with these spaces to be provided as accessible parking spaces, as per SEPP (Housing) 2021 Schedule 4, Part 1, Clause 5. In response, the development proposes two (2) car parking spaces in the form of accessible parking spaces in compliance with SEPP.

In addition to the above, the development proposes an additional four (4) standard parking spaces, resulting in a total provision for six (6) car parking spaces. This increased car parking provision is considered appropriate and will ensure all standard car parking demands are contained within the development, which would otherwise utilise on-street parking. As such, this proposed provision is considered acceptable and would result in a public benefit, thereby improving the amenity of other residents in the locality.

Accessible Parking

As mentioned above, two (2) car parking spaces are required to be provided as accessible parking spaces, both of which have been provided in compliance with SEPP. Furthermore, SEPP (Housing) 2021 Schedule 4, Part 1, Clause 5(b) requires at least one (1) parking space to be designed to enable the width of the space to be increased to 3.8 metres. In response, the development proposes a single space within the carport that is able to be widened to 3.8 metres, in compliance with SEPP.

Motorcycle and Bicycle Parking

The Blacktown DCP 2015 and SEPP (Housing) 2021 do not specify motorcycle and bicycle parking rates for seniors living developments. As such, no motorcycle and bicycle parking spaces are required or proposed.

Refuse Collection

The development proposes all refuse collection be undertaken utilising the existing on-street collection service on Pank Parade. Bin storage areas are proposed along the northern frontage of the site from which, bins would be transferred onto Pank Parade for collection.

It is emphasised that refuse collection will occur on an infrequent basis and typically outside on-street peak periods. As such, the existing on-street collection is considered appropriate and supportable, given the nature and scale of the proposed development.

Traffic Generation

Existing Traffic Generation

The TfNSW Technical Direction TDT 2013/04a (TfNSW TDT) provides the traffic generation rates for low density residential dwellings at a rate of 0.95 and 0.99 vehicle trips per hour during the morning and evening peak periods, respectively. Application of these rates to the three (3) existing residential dwelling, results in the following existing traffic generation:

- 3 vehicle trip per hour during the morning peak period (0 in, 3 out); and
- 3 vehicle trip per hour during the evening peak period (3 in, 0 out)

Proposed Traffic Generation

The TfNSW TDT 2013 provides the traffic generation rate for seniors living developments at a rate of 0.4 vehicle trips per hour during the evening peak period, noting that the morning peak period does not coincide with the network peak period. Application of this rate to the proposed 12 dwellings, results in the following anticipated traffic generation:

- 0 vehicle trips per hour during the morning peak period (0 in, 0 out); and
- 5 vehicle trips per hour during the evening peak period (4 in, 1 out).

Net Trip Generation

The above traffic generation is not however a net change over existing conditions. When taking into account the existing development, the proposed development is anticipated to result in the following net traffic generation:

- -3 vehicle trips per hour during the morning peak period (0 in, -3 out); and
- +2 vehicle trips per hour during the evening peak period (+1 in, +1 out).

This anticipated net traffic generation would equate to a single additional vehicle every 30 minutes during the evening peak period and a reduction in traffic during the morning peak period. This net traffic generation is considered minor and would have negligible impacts onto the surrounding road network.

Accordingly, the traffic generation as a result of the development is considered supportable from a traffic planning perspective, with no external changes required to facilitate the development.

➤ Access and Internal Design

Vehicular Access

The proposed development incorporates a total of six (6) car parking spaces (User Class 1A) with access onto Pank Parade, a local road. It will therefore require a Category 1 access under AS2890.1 (2004), being a 3.0-5.5 metre wide combined entry-egress access. In response, the development proposes a total of two (2) vehicular accesses, comprising:

- 5.5 metre wide combined entry-egress access for five (5) spaces within the at-grade carpark; and
- 3.0 metre wide domestic driveway for one (1) space within the carport.

These vehicular access arrangements are therefore sufficient to comply with the requirements of AS2890.1 (2004), hence considered acceptable.

Internal Design

The carpark areas generally comply with the requirements of AS2890.1 (2004), with the following characteristics noteworthy:

- All standard parking spaces have been designed in accordance with AS2890.1 (2004) User Class 1A, being a minimum width of 2.4 metres and length of 5.4 metres.
- All accessible parking spaces have been designed in accordance with AS2890.6 (2009), being a minimum width of 2.4 metres, length of 5.4 metres and provide an adjacent shared zone with the same dimensions.
- All spaces adjacent to obstructions greater than 150mm in height are to be provided with an additional width of 300mm.
- The dead-end aisle is provided with the required 1.0 metre aisle extension in accordance with AS2890.1 (2004), Figure 2.3.
- A minimum head height clearance of 2.2 metres is to be provided for all trafficable areas.
- A minimum head height clearance of 2.5 metres is to be provided for all accessible parking spaces.
- Appropriate visual sight splays have been provided at the vehicular access.
- A swept path analysis has been undertaken and provided in **Attachment 3**, demonstrating satisfactory vehicle movements.

In summary, the internal configuration of the carpark areas have been designed in accordance with AS2890.1 (2004). It is however envisaged that a condition of consent would be imposed requiring compliance with these standards. As such, any minor amendments considered necessary (if any) can be dealt with prior to the release of a Construction Certificate.

➤ **Conclusion**

On the basis of the above, the proposed LAHC seniors living development at 62-66 Pank Parade, Blacktown in our view is considered supportable.

We trust the above is of assistance and request that you contact the undersigned should you have any queries or require any further information. In the event that any concerns remain, we request an opportunity to discuss these with Council officers prior to any determination being made.

Yours faithfully,

Traffix



Neil Caga
Traffic Engineer

Encl: *Attachment 1 – Photographic Record*
 Attachment 2 – Reduced Plans
 Attachment 3 – Swept Path Analysis

ATTACHMENT 1

Photographic Record



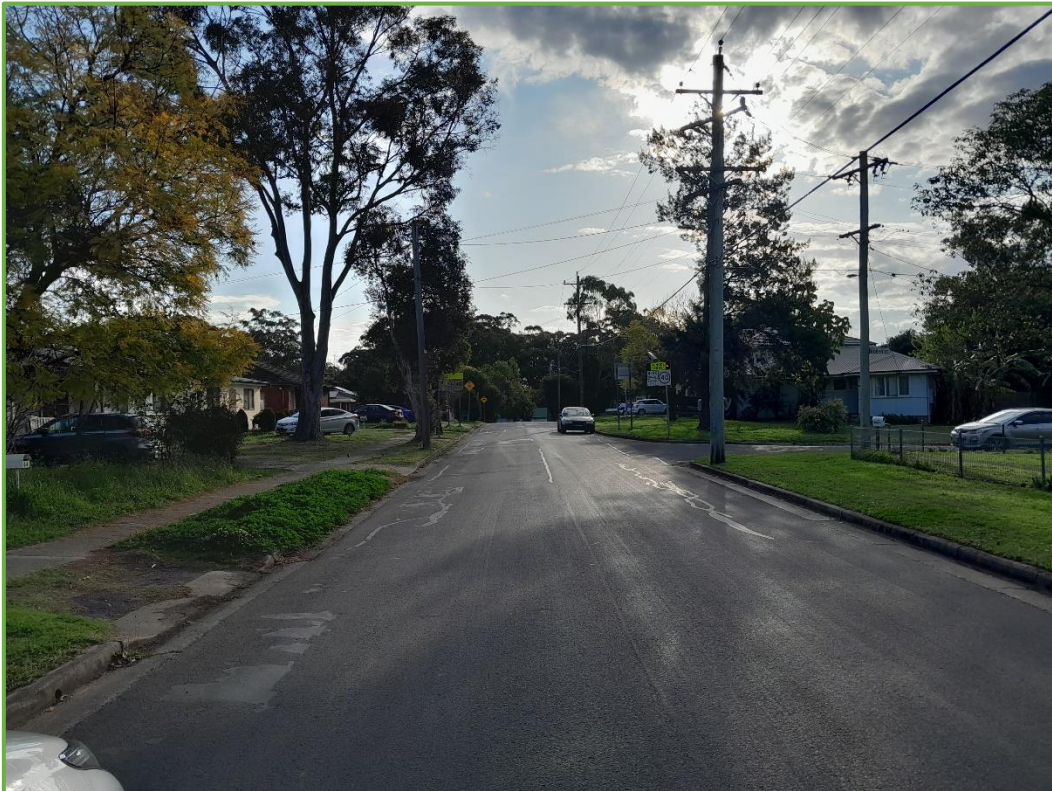
View looking south from Pank Parads towards the subject site



View looking south from Pank Parade towards the vehicular access



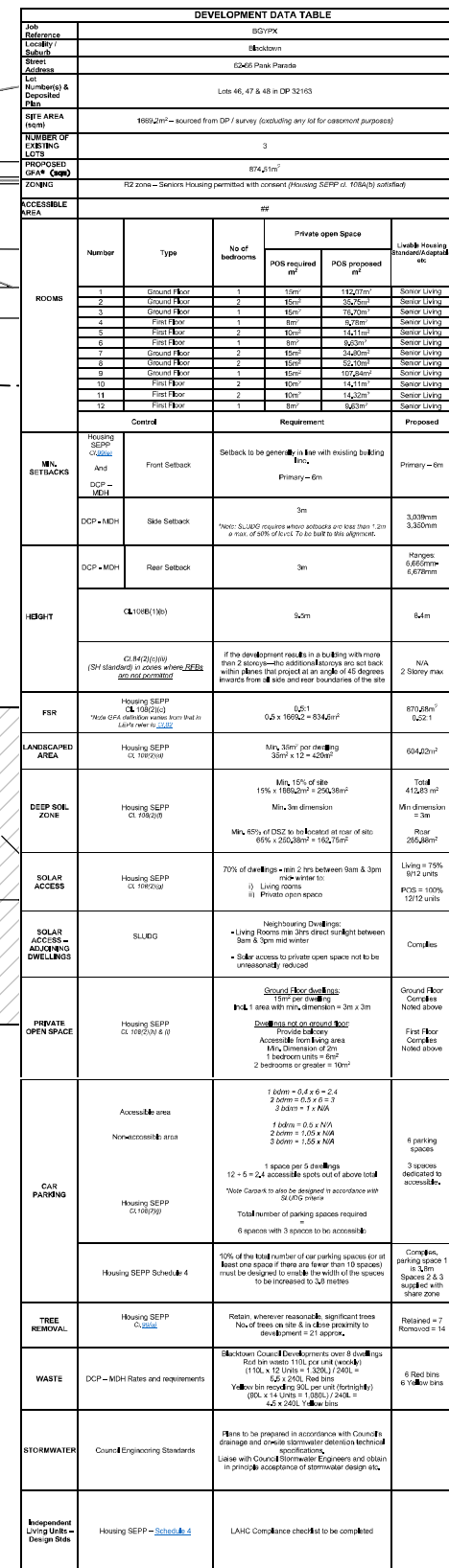
View looking east along Pank Parade



View looking west along Pank Parade

ATTACHMENT 2

Reduced Plans



Status: Part 5 Activity Submission			
Date:	Scale:	\$[a] job no:	Project
10/11/2022	1:100 @ A1	2793.22	BGYE
Stage:	Drawn:	Checked:	Approved
	JOK, DD	JOK	JOK
Drawing:	Sheet:		Rev:
DA03	4	of 14	02

ATTACHMENT 3

Swept Path Analysis



Notes:


This drawing is prepared for information purposes only. It is not to be used for construction.

TRAFFIX is responsible for vehicle swept path diagrams and/or drawing mark-ups only. Base drawing prepared by others.

Vehicle swept path diagrams prepared using computer generated turning path software and associated CAD drawing platforms. Vehicle data based upon relevant Australian Standards (AS/NZS 2890.1:2004 *Parking facilities - Off-street car parking*, and/or AS2890.2:2002 *Parking facilities - Off-street commercial vehicle facilities*). These standards allow a degree of tolerance, however the vehicle characteristics in these standards represent a suitable design vehicle and do not account for all variations in vehicle dimensions / specifications and/or driver ability or behaviour.

Rev.	Revision Note	By.	Date
A	Swept Path Analysis	NC	29-09-2022
B	Updated Plans	NC	15-11-2022
C	Updated Plans	NC	23-06-2023

Swept Path Legend

- Wheel Path
- Vehicle Body Envelope
-  Clearance Envelope (300mm)

Architect

Stanton Dahl Architects

Client	
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NSW Land & Housing Corporation

Scale / Plan Orientation



Project Description

Senior Living Development
62-66 Pank Parade, BLACKTOWN NSW 2148

Drawing Prepared By

TRAFFIX
TRAFFIC AND TRANSPORT PLANNERS

Suite 2.08, 50 Holt Street
Surry Hills, NSW 2010
PO Box 1124
Strawberry Hills, NSW 2012

Drawing Title

Swept Path Analysis
Site Plan - Vehicular Access
B85 and B99 Design Vehicles
TOP: Entry Movement
BOTTOM: Exit Movement

Drawn: NC	Checked: NC	Date: 29-09-2022
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22.030d03v01 TRAFFIX [2023-06-23] - Design Review.dwg

Project No.	Drawing Phase	Drawing No.	Rev.
22.030	DA	TX.01	C